

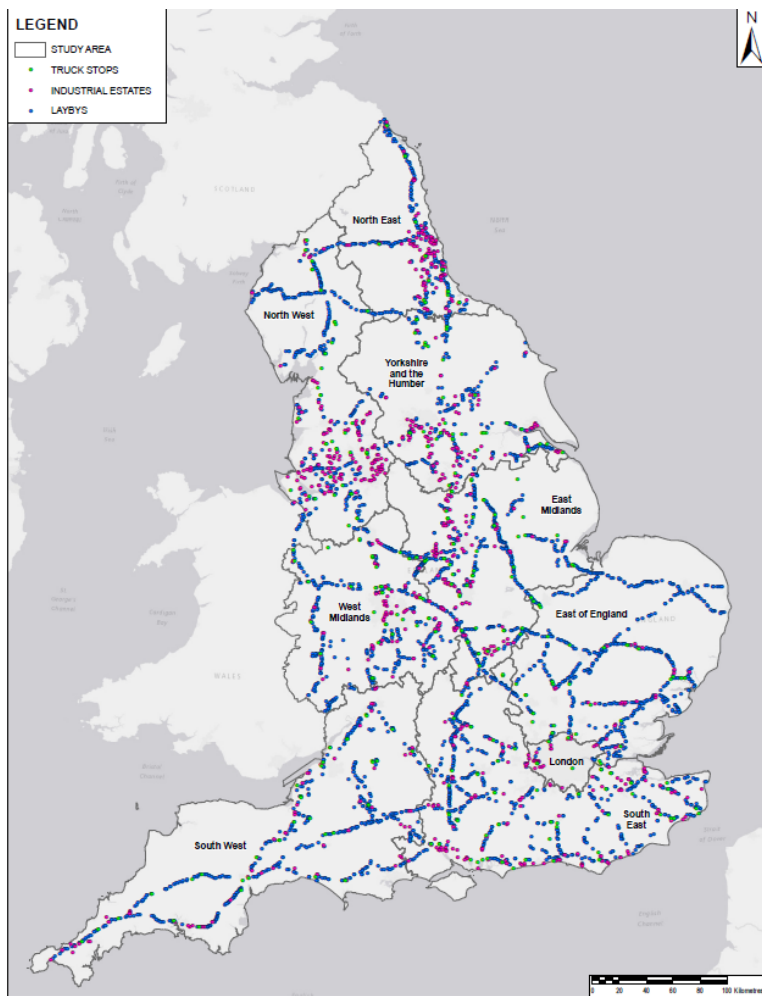
# South East roadside facilities study

May 2021

## Context to this work – DCO Risks for LTC

- The removal of the RSF from the LTC design, which gave rise to the Working Group, has given rise to the following risks for LTC at Examination which might be exploited by key Stakeholders:
  - Reduced safety, as recognised by us setting up the Working Group;
  - Lost opportunity to provide lorry parking;
  - The removal of the RSF was linked with the removal of Tilbury Junction and therefore the loss of a junction from the route
- It is anticipated that LTC will be resubmitted for DCO in Autumn 2021, and therefore Stakeholders need to be supportive of the progress of the Working Group at that time.

# Study – Planning for lorry parks 2018 - 2019



3

Aecom (using same as team as delivered DfT lorry demand study) to:

- Set out characteristics required for good, effective, sustainable and well-planned lorry parks around the SRN.
- To support better informed regional planning teams who can make better cases to and with LPAs in support of increased provision in local plans and similar.
- To be cognisant of wider long-term ambitions (e.g. *Connecting the Country* trends) and freight-targeted initiatives (e.g. SES programme).
- Relate findings of Freight Demand Survey and SRN to set out specific locational priorities.

# Activities To-Date and Next Steps

## STAGE 1

Map stakeholders

Introduce RSF working group

Carry out first trawl

- Sites on market
- Sites adjacent and accessible to SRN
- Expanded area north of M25

Refine first trawl with OD

Consider additional work

**By March 2021**

## STAGE 2

Present to directors

Present first trawl to stakeholders

**By June 2021**

## STAGE 3

Refresh & present first trawl to developers and LPAs

Meet Stakeholders

Carry out deep dive

**By End 2021**

## STAGE 4

Refresh & present deep dive to developers

Meet stakeholders

Hand over to the market

**By End 2022**

At planning pre-application stage -Developer contacts HE regional planning Team/Roadside Facilities Team and Local Planning Authority

Agree scoping, timescales and ongoing liaison. RSF to liaise with developer on DfT 02/2013 Annex B requirements and potential signing strategy

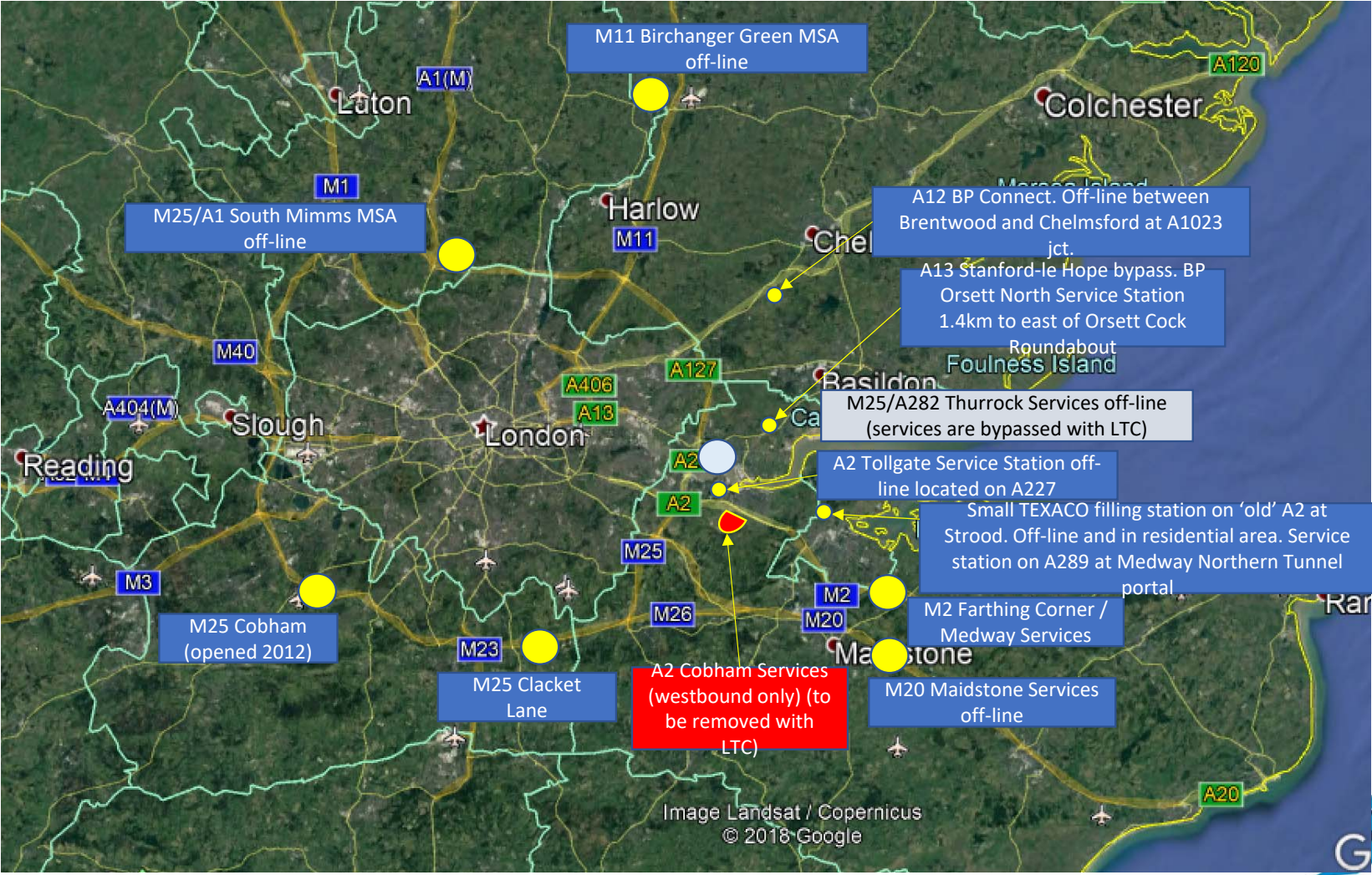
Formal planning application submitted, including traffic assessment and travel plan

As part of the planning Statutory Consultee process, regional Area team liaise with RSF to confirm development meets policy signing requirements

Planning consent determined

When construction begins developer liaises with area team and RSF on signing strategy using RSF process on new or updated signs.










# Existing RSFs

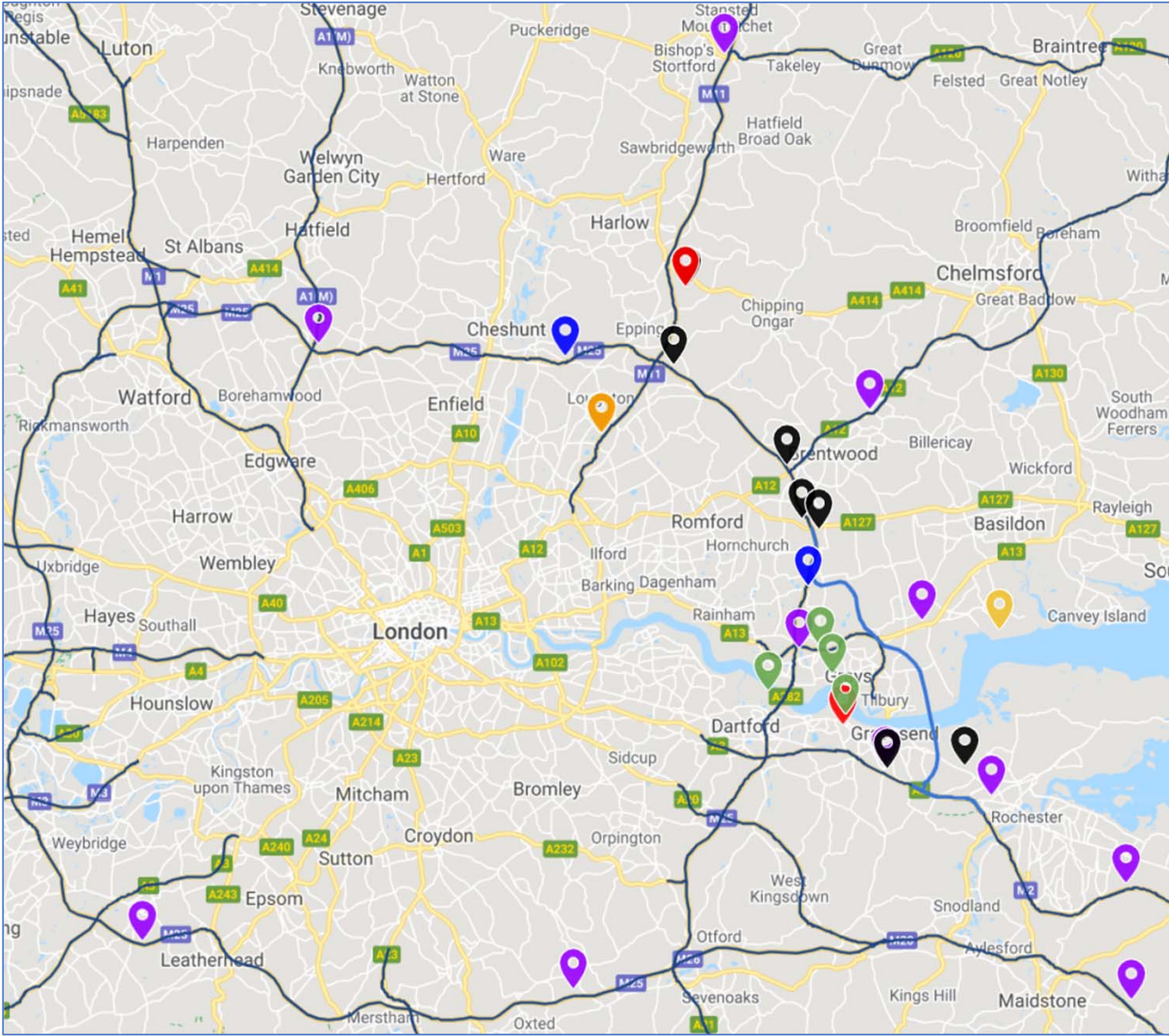


# LTC Overview of Phases 1 – 3 (May 2020 to Feb 2021)

- Phase 1 – Preliminary study into suitable sites for lorry parks that were on the open market within 14 mile radius (c. 15 minute drive time);
- Phase 2 – Study focus extended to consider suitable sites within 5km of LTC and included site surveys – 5 shortlisted sites identified;
- Phase 3 – Expanded study area further north of M25 and assessment of North Weald Bassett Airfield and additional site surveys - 5 further sites identified.

# MSA/Lorry Parking Update

-  AECOM Market Sites
-  MHCLG Operational Site
-  MHCLG Reserve Site
-  HE Owned Site
-  Existing Road Side Facilities
-  AECOM Identified Site
-  AECOM Shortlisted Site
-  Lower Thames Crossing Route
-  Strategic Road Network



## Headline findings

- Have been able to develop a shortlist of opportunities worthy of further consideration.
- Although an independent review, some of the sites AECOM have produced are the same as LTC looked at when thinking about including a site in their scheme.
- There is a differentiated market between lorry parks and MSA's so appropriate to look in isolation as well as together.
- No obvious or straight-forward location. Less known market interest in this quadrant of M25.
- More opportunities north of Thames than south.
- More opportunities away from LTC than on it, with LTC's current design.

# Next steps & proposals for further work

## Purpose:

- To update on work undertaken to date.
- To note challenges to bringing forward facilities in corridor.
- To discuss role of Highways England in addressing issue of provision of roadside facilities, including lorry parking.
- To agree next stages, including priorities & resourcing.

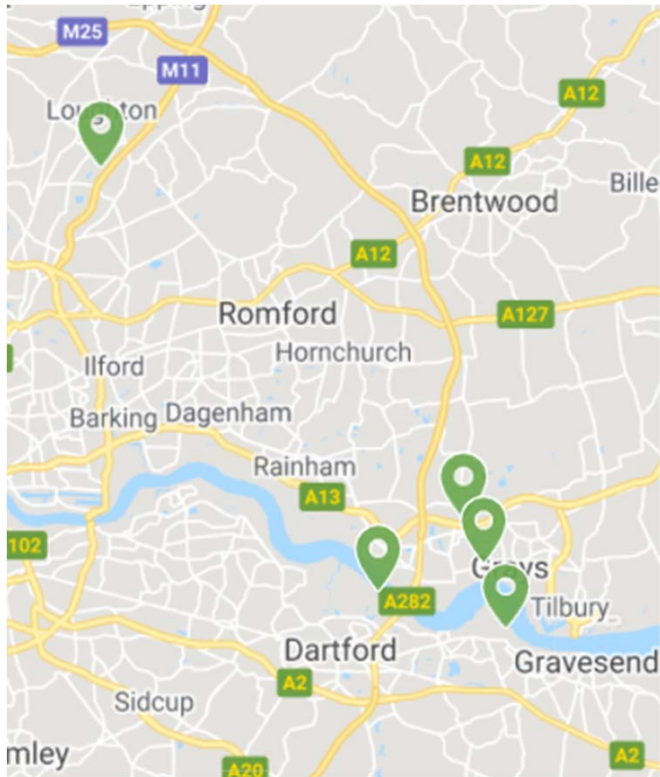
## What's the ask?:

- Confirmation that work to date is useful, and role of working group is making a difference.
- Agree where ownership (and associated resource requirements) sit for this and next stage of the work?
- Agree the overriding objective of this work area?

- a) Review why there appears to be less demand from lorry park / MSA operators for bringing forward sites in the north-east quadrant of the M25 – delivered internally;
- b) Progress review of sites between junctions NE quadrant M25 – subject to funding;
- c) Explore other possibilities:
  - i. Reuse / adaptation of LTC construction compound(s);
  - ii. Redevelop existing Thurrock services into lorry park, and relocate service area to other suitable location;
  - iii. Including within Tilbury Link Road, wider plans – including those linked to Thames Freeport and other international gateways;
  - iv. Chigwell, as a surplus site in our ownership.
- d) Update on electric vehicle charging needs in the region;
- e) Roadside facilities and lorry parking as a core requirement of scheme development and design, excluded by exception rather than other way round.
- f) Influence Local Plans in terms of site allocations supporting provision of facilities.
- g) Stakeholder engagement, including international gateways and service operators, ongoing through summer 2021.

# Overview of Work Undertaken

# Phase 1 - Study of Sites on the Market



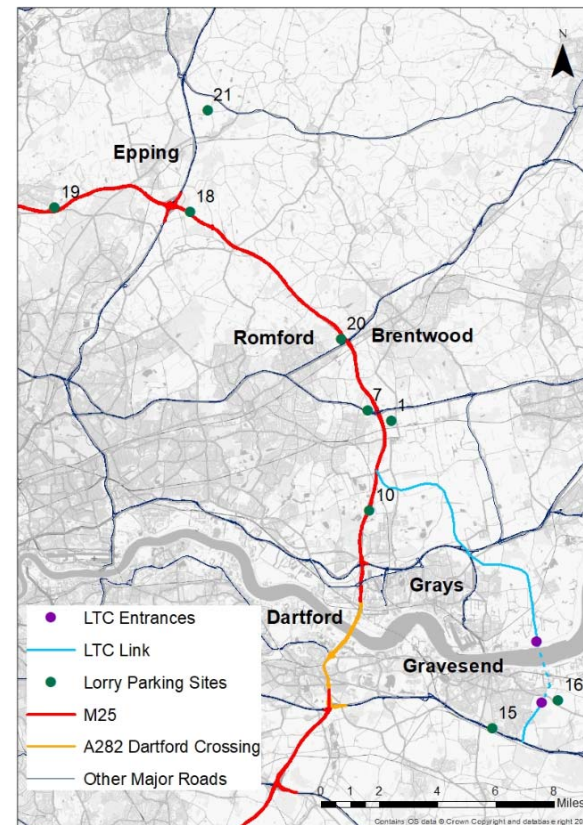
Preliminary study into any suitable sites for lorry parks that are on the open market, conducted summer 2020.

Five sites were shortlisted, but each were discounted for various reasons including congestion risk, proximity to homes and distance from LTC.

- Littlebrook Power Station, Dartford
- Lower Road, Gravesend
- Lakeside Coach Park, Grays
- Fiddlers Reach, Grays
- Chigwell, Essex

## Phase 2 and 3, Long list of sites - Overview and location

| Site label | Site location                          | MSA or Lorry Park | Phase |
|------------|--|-------------------|-------|
| 1          | <b>Great Warley (Junction 29, M25)</b> | MSA/Lorry Park    | 2     |
| 7          | Folkes Lane (Junction 29, M25)         | Lorry Park        | 2     |
| 10         | <b>Dennis Road (Junction 29, M25)</b>  | MSA               | 2     |
| 15         | <b>Watling Street (A2)</b>             | Lorry Park        | 2     |
| 16         | <b>St Mary's Chalk (A226)</b>          | MSA               | 2     |
| 18         | Hobbs Cross (Junction 27, M25)         | MSA               | 3     |
| 19         | <b>Honey Lane (Junction 26, M25)</b>   | Lorry Park        | 3     |
| 20         | Promac Commercial (Junction 28, M25)   | MSA/Lorry Park    | 3     |
| 21         | North Weald Airfield (A414)            | Lorry Park        | 3     |



## Discounted sites

| Site no. | Site                                 | Rationale  |
|----------|--------------------------------------|--|
| 1        | Great Warley, Junction 29 M25        | <ul style="list-style-type: none"> <li>The site has been highlighted for an Enterprise Park in Brentwood Council's local plan</li> <li>Major junction modification required to facilitate an MSA</li> </ul>                                    |
| 7        | Folkes Lane (Junction 29, M25)       | <ul style="list-style-type: none"> <li>Tight turn to enter site</li> <li>Lack of turning space</li> <li>Size of site (1 ha)</li> </ul>   |
| 16       | St Mary's Chalk, A226                | <ul style="list-style-type: none"> <li>Site is close to southern LTC tunnel portal</li> <li>Site is adjacent to the RAMSAR site and any development here would impact that area</li> </ul>   |
| 18       | Hobbs Cross (Junction 27, M25)       | <ul style="list-style-type: none"> <li>Safety issue – close to existing slip road</li> </ul>   |
| 20       | Promac Commercial (Junction 28, M25) | <ul style="list-style-type: none"> <li>Tight entrance and exit to site</li> <li>Vehicles travelling South would need to go back up around the junction increasing traffic on the north west point of the junction</li> </ul>                   |
| 21       | North Weald Airfield (A414)          | <ul style="list-style-type: none"> <li>Remote location</li> <li>Narrow and tight approach roads</li> <li>Vehicles travelling South would need to go back up to M11 and travel back down</li> <li>Distance to LTC and off main route</li> </ul> |

# Remaining Sites



Honey lane, M25 J26

- 13-26 ha
- Potential 1214 lorry spaces



Watling street, A2

- 3.2 ha
- Potential 150 lorry spaces



Dennis Road, M25 J29

- 24.9 ha
- Potential 873 lorry spaces